

West Norriton

WEST MAIN STREET VISION PLAN



WEST NORRITON TOWNSHIP

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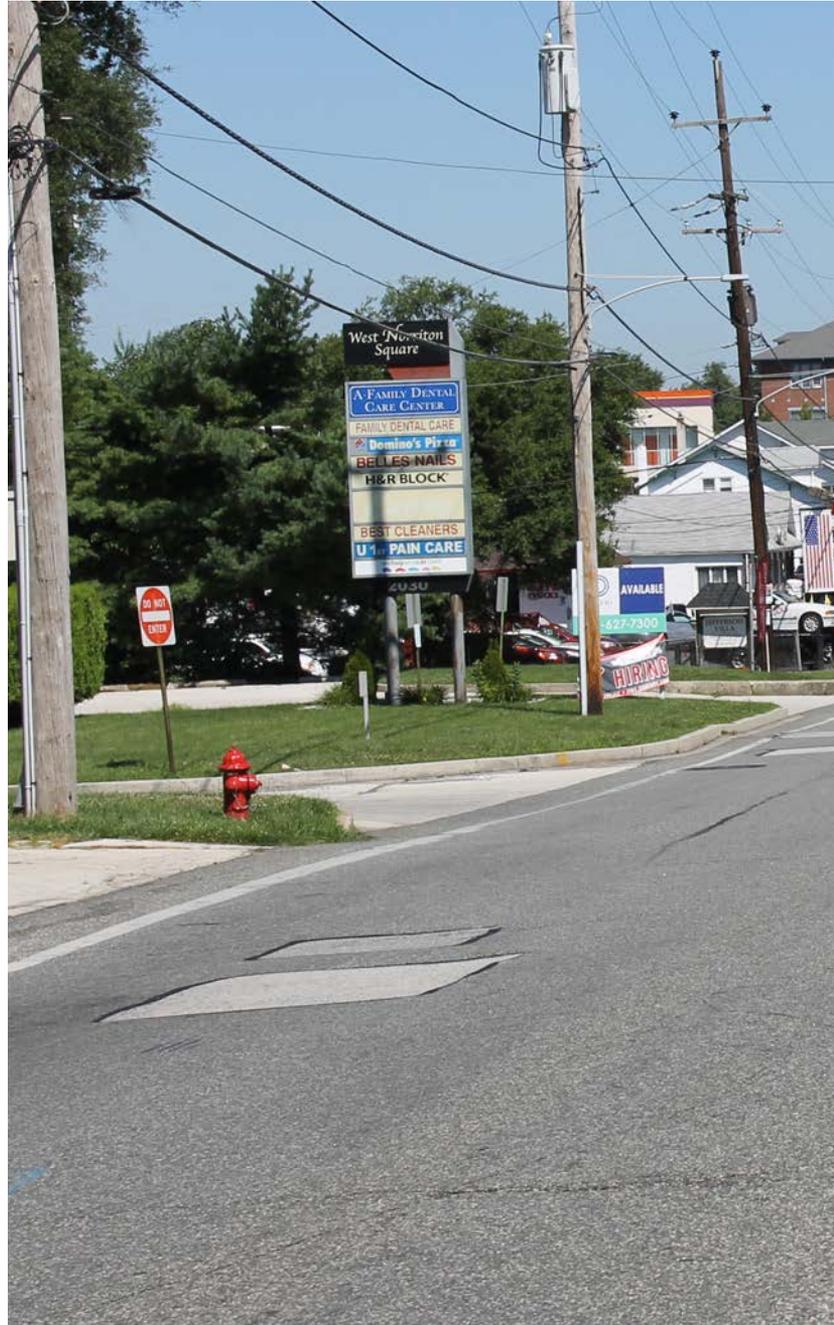
SPECIAL THANKS

**To the businesses, property owners, and
West Norriton residents who provided
feedback and direction on the plan.**



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CHAPTER 1



INTRODUCTION

PURPOSE

The West Main Street Vision Plan is intended to outline a cohesive vision and provide design guidelines to manage future development and transformation along the West Main Street Corridor in West Norriton Township. This plan explores the full length of West Main Street in the township, spanning from the western boundary at Trooper Road to the eastern boundary at Forrest Avenue. This roadway section of just under two miles is centrally located in the township and is the major east-west road network that belongs to the larger Ridge Pike arterial route through southern Montgomery County connecting Philadelphia to Pottstown.

Development along the corridor has occurred in a piecemeal fashion, resulting in parcels with irregular building setbacks, varying façade treatments, undefined curb cuts, and limited pedestrian amenities. Beginning in 2016, the West Norriton Planning Commission, in partnership with the Montgomery County Planning Commission, began examining the corridor to make recommendations for enhancing its commercial and mixed-use development opportunities, and to create a more pedestrian-friendly road network. Public outreach was conducted through a series of public workshops, online surveys, and meetings with the local business community. This document represents the results of this outreach, and reflects the desires of the community for both short-term improvements to existing properties and long-range goals for future redevelopment opportunities. Implementation steps are provided to help make this plan a reality, which include recommendations for amendments to the zoning code and subdivision and land development ordinance, and the identification of grants to fund the transformation of properties and businesses along West Main Street.

GOALS

- Create design guidelines to establish uniform setbacks, curbing, sidewalks, and other street treatments to be implemented in the event of redevelopment.
- Establish near-term improvement opportunities for existing properties to increase pedestrian infrastructure and improve vehicular circulation patterns.
- Identify updates to be made to the West Norriton zoning and subdivision and land development ordinances.
- Identify targeted outcomes to be achieved through Capital planning and grant funding.



Relationship to the Township Comprehensive Plan

In 2007, the township identified the need to establish a special corridor for properties along West Main Street. The Revitalization and Traffic Management Overlay (R-TMO) District was created as a result of this need to address traffic safety, shared access, pedestrian and vehicular connections, the coordination of development between properties, and streetscape enhancement. The township's comprehensive plan update in 2010 brought specific attention to this effort, and made additional recommendations to create landscape buffers around parking areas with street trees and attractive street lighting. To address roadway safety concerns, the comprehensive plan also recommended replacing center turning lanes with landscaped medians to better control the flow of traffic and create protected left turn lanes at designated intersections. Specific implementation steps for "Main Street Revitalization" listed in the comprehensive plan include:

- Rebuild/construct new sidewalks at safe locations along West Main Street and Egypt Road.
- Encourage the revitalization of obsolete and underused properties to stimulate the modernization of existing businesses and growth of new businesses.
- Install street trees, pedestrian furniture, and crosswalks along West Main Street. Protect those buildings having historical and noteworthy period architecture.
- Develop improved parking areas by improving interior circulation and encouraging shared access from Main Street and secondary access between adjacent businesses. Enhance the environmental quality of retail properties through additional landscaping and the use of best management practices for stormwater management.

This vision plan builds off of previous work to formalize past recommendations into the design guidebook, and identify amendments to the zoning text to strengthen standards to ensure this vision of a cohesive corridor is created as redevelopment opportunities arise.

PLAN DEVELOPMENT PROCESS

The West Main Street Vision Plan is the result of a multi-year study of the corridor involving numerous public engagement events and extensive coordination with township staff, the West Norriton Planning Commission, and the West Norriton Board of Commissioners. A community survey was administered in 2016 which received 207 responses and a public meeting was held following the close of the community survey on June 20, 2016 at Chap's Taproom. Survey responses indicate that although pedestrian amenities ranked highly for "needs improvement", residents are generally more concerned with traffic, business diversity, building appearance, and landscaping improvements to beautify the corridor.

A business and property owner meeting was held in 2018. A letter of intent was sent out to all business owners and property owners along the corridor with a follow-up communication in advance of the meeting. The primary goal of this meeting was to develop an understanding with the business community and property owners of what their desire and ability would be to implement "interim" recommendations. This meeting attracted about 20 attendees representing a variety of businesses along the corridor.

STUDY AREA

The study area for the West Main Street Vision Plan (indicated in the dashed box, at right) includes all properties with frontage on West Main Street from Trooper Road to Forrest Avenue. An analysis was conducted of existing conditions, land uses, and future development potential along the corridor to create a tailored approach to design recommendations in this guidebook.

The area along West Main Street between Schuylkill Avenue and Montgomery Avenue is considered in this document, but no major design recommendations are proposed for this section of the roadway, as it is primarily privately-owned residences and is currently served by sidewalks on the north side of the street.





CHAPTER 2



EXISTING CONDITIONS

WEST MAIN STREET CORRIDOR

West Main Street is a two lane roadway in each direction from the township's eastern edge at Forrest Avenue to the Egypt Road/ Jefferson Avenue intersection. West of there, the roadway narrows to one lane in each direction with shoulders on either side and a shared center turning lane. Access points and curb cuts for driveway entrances along the corridor are generally not coordinated with adjacent properties, resulting in wide and open curb cuts that are not efficient in managing traffic flow. Sidewalks are not present along a majority of the non-residential property frontages which limits pedestrian mobility and accessibility, and decreases pedestrian safety.

Sidewalks

There are no sidewalks on the south side of West Main Street from Forrest Avenue to Jefferson Avenue except for the corner property at Forrest Avenue. Sidewalks are present on the north side of West Main Street from Forrest Avenue to Jefferson Avenue but do not have, for the most part, any landscaped or bricked area to serve as a buffer between the sidewalk and vehicular traffic lanes. Along some blocks, the sidewalks are separated from the roadway by a vegetated landscape strip, referred to in this document as a "verge." Verges are important, especially along roadways with higher speed limits because of the increased separation distance they provide between pedestrians and moving vehicles. Not only do verges create greater physical distance for improved safety, but they also improve



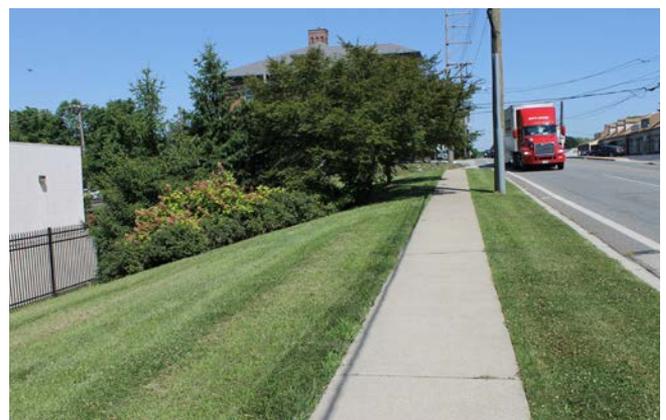
The sidewalk network is incomplete where it terminates at the edge of a vacant parcel.

the perceived sense of safety and overall comfort while walking on the sidewalk. Utility poles and other infrastructure like fire hydrants can be located in the verge, which leaves the sidewalks clear of obstructions to make it more passable for pedestrians, strollers, and wheelchairs.

West of Egypt Road, only some of the properties have curbing and sidewalks. Parcel sizes are generally wider and the dominant land use shifts to primarily commercial uses featuring auto-oriented businesses, dining establishments, and small strip-mall developments. On the south side of West Main Street, Jeffersonville Golf Club occupies a large parcel with roadway frontage between School Lane and Rose Avenue. This property does not have any sidewalks or side path features. A vacant parcel adjacent to Centennial Park, located at the intersection of Burnside Avenue and Marshall Street, also does not have a sidewalk but is adjacent to a car dealership that has full sidewalks along its frontage with



No sidewalks are present along the property frontage of the Jeffersonville Golf Club.



Some properties, like the self-storage facility, have existing sidewalks buffered by a verge.

a landscaped verge and curbing. Newer developments have more consistently included verges between the sidewalk and the curb, such as the corner parcel at Schuylkill Avenue, and the redevelopment projects on both the north and south side at Trooper Road.

Few pedestrians can be seen walking along West Main Street despite the fact the corridor offers a number of service-oriented business, grocery shops, medical offices, and restaurants within proximity to a large residential population. The commercial district could take better advantage of its location if pedestrian safety and connections to the businesses from the residential developments were improved.

Access Management

A number of parcels have little to no defined vehicle entrance areas, which enables motorists to enter and exit the property at any point. The lack of curbing also creates the condition where there is no separation from the roadway or shoulder. This scenario results in an unsafe environment for drivers and pedestrians. Parking is also often located in the front of businesses, which can further exacerbate the potential for vehicular conflict as cars are pulling into and out of parking spaces in the same area as cars are maneuvering into and out of the site.

Few properties have shared parking lots: most businesses have independent parking areas separated from neighboring properties with striping, curbs, or landscaping. Shared parking allows businesses to combine parking areas to reduce the number of curb cuts, and potentially reduce parking demands if the businesses operate during different times of the day. A bank, for example, will generate parking needs during the day while a dinner restaurant will only generate parking needs in the evenings. Two businesses have a shared parking agreement: Chap's Taproom and the Fine Wine and Good Spirits store. This agreement has allowed them to operate using one shared curb cut. Directional arrows painted on the drive aisles direct the flow of traffic and represent a good example of access management.

Between Montgomery Avenue and Schuylkill Avenue, the land uses along West Main Street are predominantly single family detached residential. Some of the homes have driveway curb cuts along West Main Street while others have alley access or driveway access off of the side streets. Residences between Montgomery Avenue and Hancock Road take driveway access off of Hartranft Drive which runs parallel to West Main Street and is separated from the main roadway with a landscaped buffer.



Chap's Taproom and the state store have a shared central driveway curb cut and parking areas which reduces the number of curb cuts along the corridor and enables the creation of a verge and sidewalk area.

Streetscape

There is a range of parcel sizes, depths, and lot widths along West Main Street. Development on these properties has been limited by these dimensions and has resulted in irregular building setbacks and arrangements of parking, sidewalks, and landscaping features. Front yard setbacks vary from less than 20 feet to greater than 50 feet. Many properties have parking located in front of the buildings. Shallow setbacks with no turnaround space requires vehicles to pull out into the travel lanes to enter and exit the parking stalls. Parking in front of the building often occupies space that would otherwise be used for landscaping and sidewalks.

Utility poles and power lines are present on both sides of West Main Street, which occupy space where sidewalks would be located and create visual clutter. The formalization of verges would allow utility poles to be located within that area to keep sidewalks clear. Where verges are not present, utility poles are often placed within the sidewalk, limiting movement and posing barriers for ADA accessibility.

Limited landscaping is planted along the roadway, largely because of the location of parking areas and conflicts with overhead utility lines. Existing verges are limited in width and are generally not suitable for planting shrubs or trees. Some properties, such as those near the Trooper Road intersection have larger landscaping areas between the sidewalk and the parking areas which allows for greater landscaping options.



Utility poles on the north side of West Main Street occupy the area where sidewalks would be located.

Recent Development

Few new developments have been built along the corridor in recent years. The most recent development occurred at the intersection of West Main Street and Trooper Road with the construction of the Citadel Bank and Wawa. This property was built with full sidewalks and a landscaped planting area separating the curb from the sidewalk, and the sidewalk from the parking area. Across the street from this development, the Burger King restaurant recently underwent a renovation that included new landscaped areas, and a reconfiguration of the parking area and drive-thru lanes. On the Lower Providence Township side of the Trooper Road intersection, a new grocery store incorporates complementary landscaping elements.

These developments exemplify many design features the township wishes to replicate throughout the corridor, including landscaped buffers, full sidewalk networks, and decorative streetscape treatments and lighting. While deep setbacks like those present in these developments are not feasible for all properties along the corridor, these properties should serve as a template for future redevelopment to create a cohesive and connected appearance for the entire West Main Street corridor.



Recent development at the Trooper Road intersection created large landscape areas.

EXISTING ZONING

There are seven zoning districts governing land uses along the West Main Street corridor, the majority of which permit commercial and retail uses. West Main Street from Forrest Avenue to Montgomery Avenue and from Schuylkill Avenue to Trooper Road are zoned Commercial (C). The south side of West Main Street between Trooper Road and Lawn Avenue is zoned Limited Commercial (LC). The Revitalization and Traffic Management Overlay District (R-TMO) follows the boundaries of the C and LC zoning districts except for a few split-zoned parcels between Egypt Road and School Lane which are zoned C and R-2 Residential. For those properties, the overlay boundary maintains a setback distance from the roadway in alignment with the parcel boundaries to the west of School Lane. Outside of the Revitalization and Traffic Management Overlay District, the corridor is almost entirely residential.

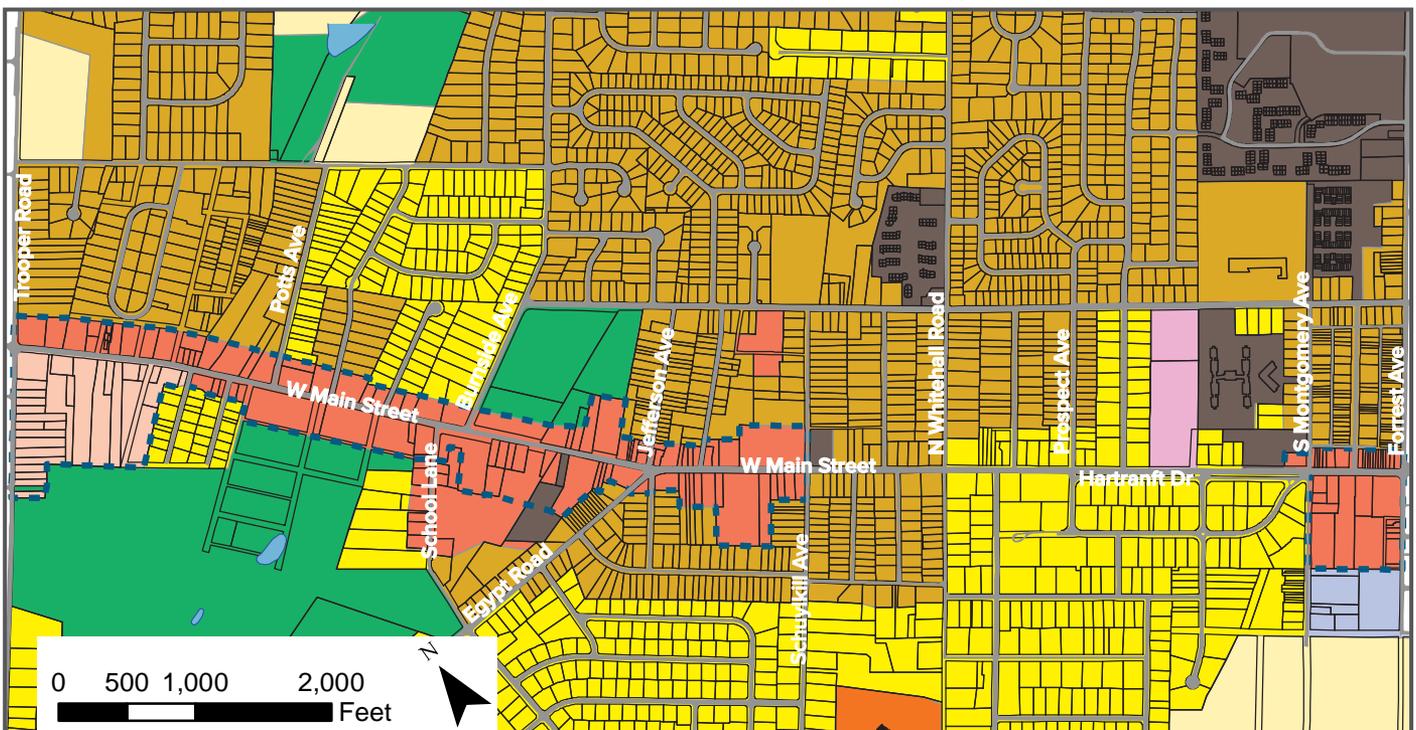
Three properties are zoned Apartment (A). One of those properties, located west of Egypt Road, is surrounded by commercial zoning but it has limited frontage along West Main Street which serves as the access drive for the

development. The rest of the properties with frontage along West Main Street are either zoned R-1 or R-2 residential. One property is zoned Business & Professional (B-P) that spans the whole block north of West Main Street to Marshall Street.

West Norriton Zoning

- R-A Rural Residential
- R-1 Residential
- R-2 Residential
- R-3 Residential
- A Apartment
- RR Retirement Residence
- C Commercial
- L-C Limited Commercial
- LC&I Limited Commercial & Industrial
- I Industrial
- B-P Business & Professional
- RE Recreational
- R-TMO District

Existing Zoning Map



EXISTING LAND USE

Within the C and LC zoning districts, the predominant land uses are retail or mixed-use. Businesses within those zoning districts include retail shops, restaurants, grocery stores, and financial institutions. Auto-oriented businesses, including car sales and auto mechanics, are also common along the corridor. One of the largest non-retail uses in the C zoning district is a self-storage facility, designated as an industrial use. Jeffersonville Golf Course is designated as a public open space use, and occupies a significant amount of frontage between School Lane and Rose Avenue. While most properties have been developed, a few vacant parcels exist. One is located on the northeast corner of Burnside Avenue and West Main Street, and another is located on the northeast corner of Schuylkill Avenue.

Existing Land Use Map

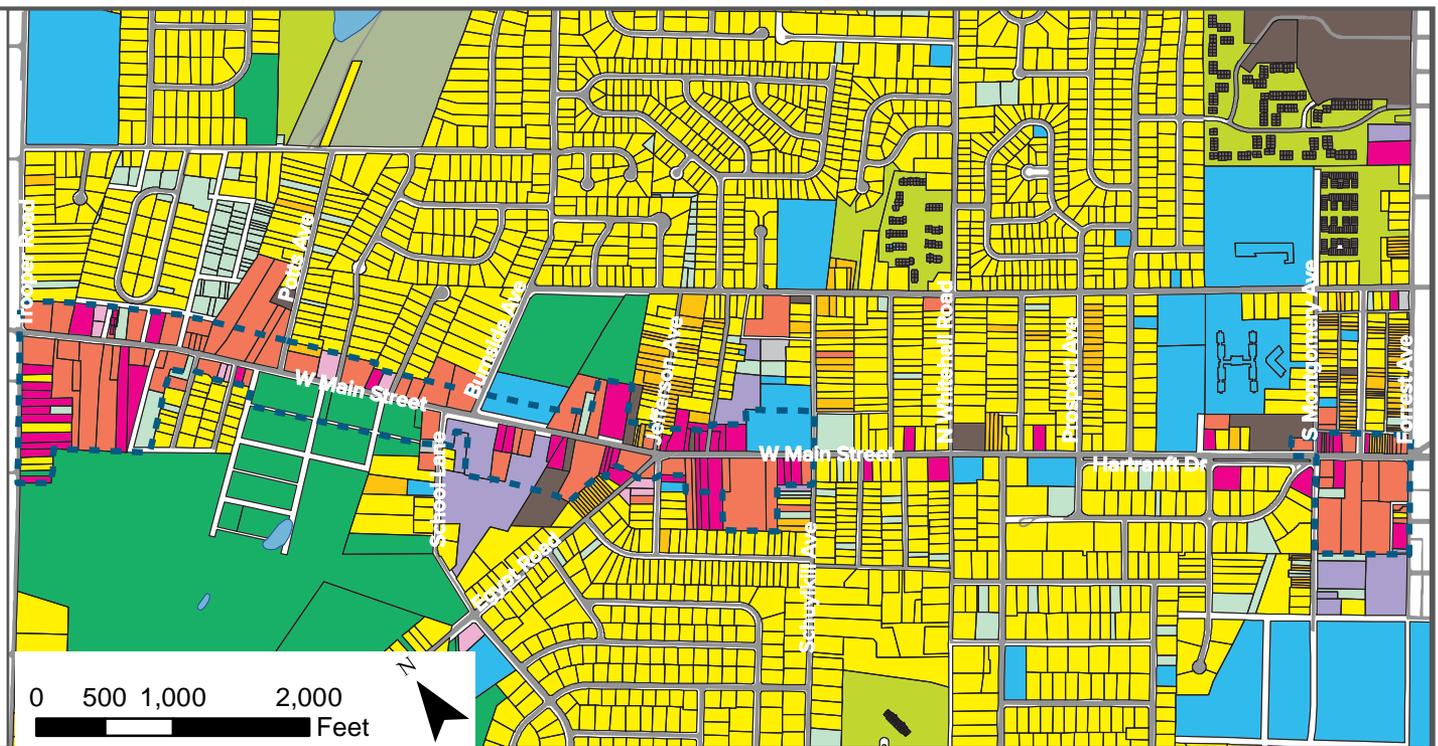
West Norriton Land Use

-  Multifamily
-  Single Family Attached
-  Twin / Duplex
-  Single Family Detached
-  Country Residence
-  Mixed Use
-  Retail
-  Office
-  Industrial
-  Institutional
-  Utilities
-  Undeveloped
-  Public Open Space
-  Private Open Space
-  Agriculture
-  RMTO District

The northwest corner of that same intersection is designated as an institutional use and contains a parking lot and a cemetery for the Jeffersonville Presbyterian Church. Another substantial institutional use is the property that is zoned B-P, and is the location of the Montgomery County Intermediate Unit Early Learning Academy.

Land uses between Schuylkill Avenue and Montgomery Avenue are predominantly single family detached residential, but also include some multifamily uses in apartment complexes. A few of the single family residential properties are designated as mixed-use, and are likely small businesses being operated out of a residential house.

Residential neighborhoods immediately adjacent to the West Main Street corridor are arranged around a more dense and gridded street network east of Jefferson Avenue, and a more curvilinear street network west of Burnside Avenue. Numerous residential streets link directly into West Main Street which provides connected access for residents to travel to the commercial district and the public transportation network. Although there is an extensive residential population in the immediate vicinity of the commercial corridor, the lack of safe pedestrian access and the variety of available businesses may limit the frequency with which local residents walk to local businesses. Increasing the business mix to include more retail shops and restaurants may attract more local residents.



TRANSPORTATION AND SAFETY

Crash data shows many automobile crashes on West Main Street occurred at or near intersections, with the highest concentration of crashes occurring at the Egypt Road intersection. The majority of crashes were categorized as either “rear-end” or “collision”, both of which involve body damage to a car, and only a few were categorized as “head-on”. Crashes involving pedestrians were much fewer in number, but occurred exclusively around the Egypt Road intersection and the Montgomery Avenue intersection.

Speeding was frequently cited in the community survey as a major safety concern along the corridor, along with traffic congestion that made turning movements onto and off of West Main Street challenging. The speed limit is posted at 40 MPH, but survey respondents indicated they had encountered drivers traveling at much higher speeds.

Traffic volumes on West Main Street lead to heavy congestion, especially during the afternoon rush hour. Because the roadway narrows down to one lane in each direction west of Egypt Road, traffic congestion is heaviest between there and Trooper Road.

Traffic congestion creates challenges when attempting to make left turning movements onto and off of West Main Street. There are also no traffic lights between Egypt Road and Trooper Road, which further exacerbates the challenges of drivers making turning movements. Survey respondents indicated the intersection at Burnside Avenue was particularly problematic when making turns onto West Main Street. The average daily trip (ADT) count for West Main Street between Forrest Avenue and Egypt Road is between 26,000 and 27,000 vehicles. The ADT drops west of Egypt Road likely because many vehicles turn off of West Main Street at Egypt Road, which leads to Route 422. Between Egypt Road and Burnside Avenue, the ADT is 11,000, and between Burnside Avenue and

Trooper Road the ADT is 16,000. Burnside Avenue alone has an ADT of 6,400 between West Main Street and Marshall Street, and represents a major travel route for drivers.

PennDOT owns West Main Street, which means that all properties that take access from the roadway must obtain a highway occupancy permit (HOP) from the state to modify curb cuts and driveway access points. Additionally, any roadway improvements to the median, crosswalks, traffic lights, and signal timing is managed by PennDOT. In 2018, PennDOT announced a roadway project for the West Main Street (SR 3009) corridor from Egypt Road to Forrest Avenue under the Highway Safety Improvement Program (HSIP). This project would include road widening to create new left turn lanes at five intersections, modify the left turn signal phases, and increase the all-red intervals at the Egypt Road intersection to allow for longer pedestrian crossing times. This project will have significant impacts on the streetscape throughout this area of the corridor, and the township should work with PennDOT when this project is pursued to ensure resulting development incorporates the guidelines recommended in this study.



Heavy traffic volumes create long queues for the turning lanes at Burnside Avenue.

PUBLIC TRANSIT

West Norriton Township is serviced by suburban bus routes operated by SEPTA. Bus Route 131 travels between the Norristown Transportation Center and the Parke Pointe Business Center in Audubon. It has limited stops along West Main Street between Egypt Road and Schuylkill Avenue. Route 91 travels on West Main Street but the only stop it makes is at West Main Street and Forrest Avenue. Route 93 has the most number of stops on West Main Street and travels between the Norristown Transportation Center and Pottstown.

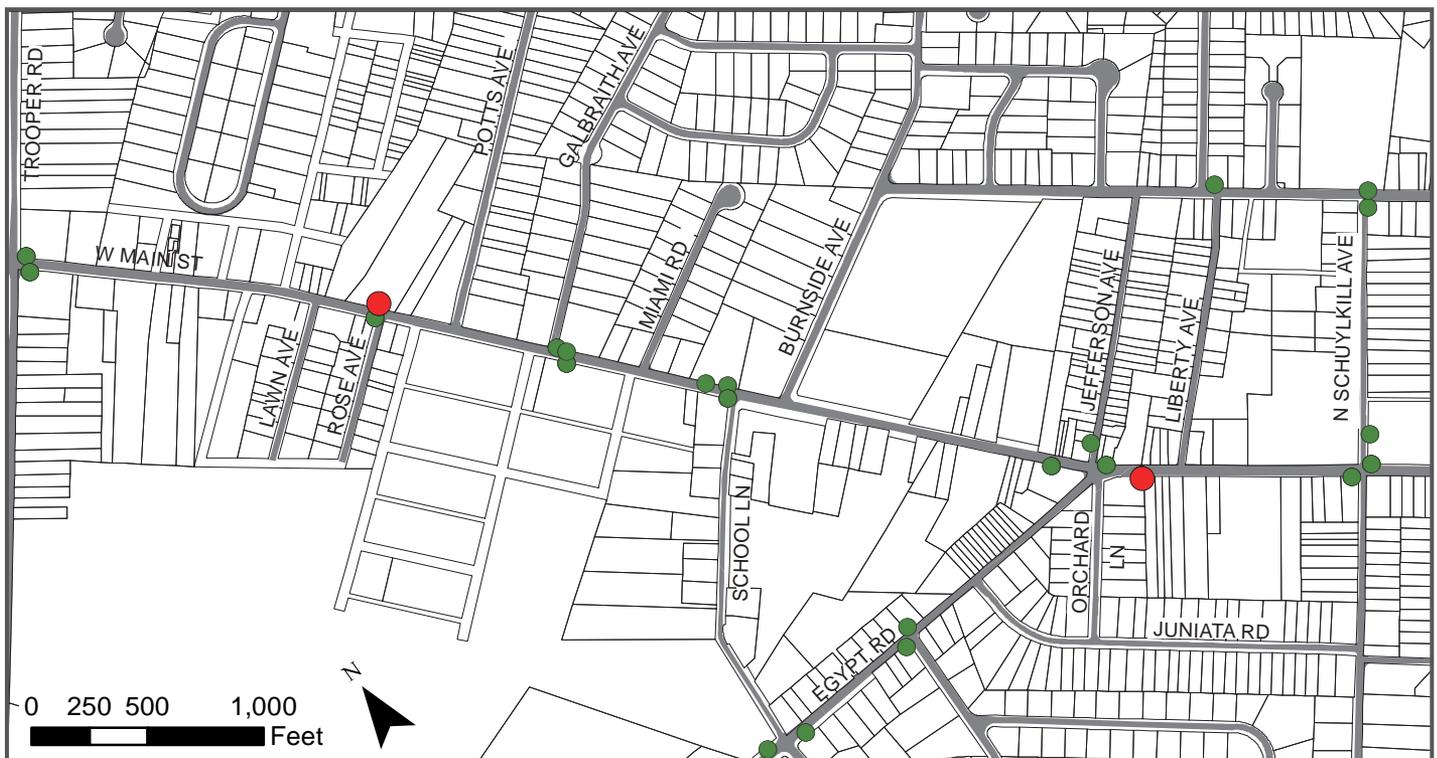
There is limited transit infrastructure on West Main Street. Bus stops are designated only by signs posted on utility poles, but no bus shelters or benches are present at any of the stop locations. These amenities are beneficial to provide riders with shelter from the weather as well as seating, and increase overall visibility and safety for both riders and drivers. Based on ridership data and available room for improvement, bus stops that include shelters could be installed at defined locations, indicated on the map below.

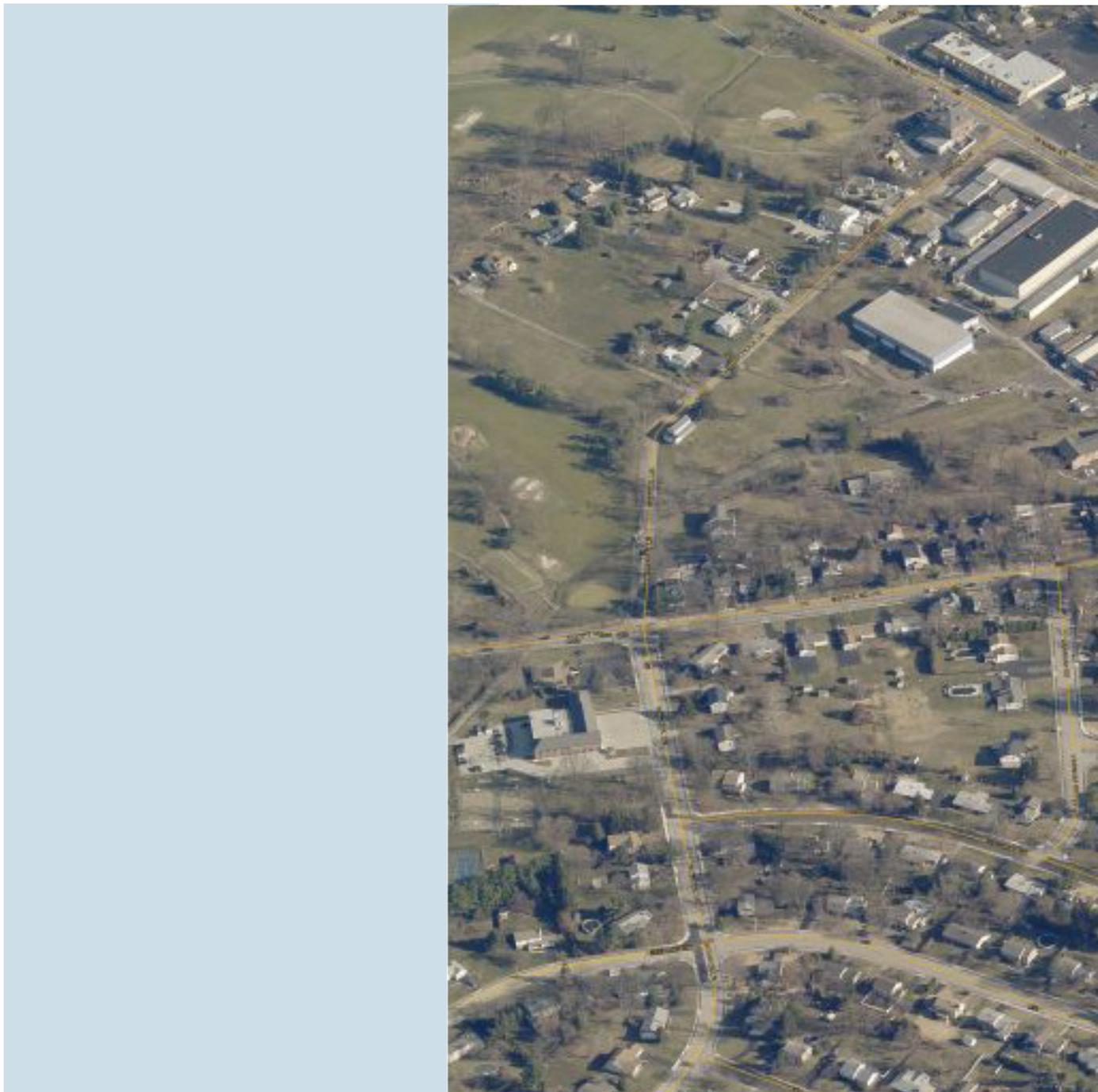


Limited pedestrian infrastructure and amenities at bus stops may serve as a deterrent to transit ridership and create potentially dangerous conditions for riders.

Transit Map

- Proposed Improved Bus Stop
- Existing Bus Stop







CHAPTER 3

CORRIDOR ANALYSIS

West Main Street has different development patterns along the length of the corridor that require unique design approaches, rather than a one-size-fits-all approach. To account for these varied conditions throughout the study area, an analysis was conducted of existing parcel configurations, building setbacks, driveway access points, transit stops, landscaping, parking, and sidewalks to determine appropriate and context-specific recommendations for future design guidelines and zoning standards.

EXISTING SETBACKS

Building setbacks for properties along the corridor vary from less than 20 feet to over 50 feet. Deeper setbacks are mostly found on properties located between Potts Avenue and Trooper Road, while setbacks around the Egypt Road intersection and around Montgomery Avenue tend to be closer to the roadway. Some properties around Egypt Road front directly on the sidewalk, with no setbacks at all. Shallow setbacks are generally found in more urban downtown centers or village areas, while deeper setbacks are more common in highway-oriented commercial centers. The mix of setback depths creates a disjointed pattern of development throughout the corridor.



Properties near the Egypt Road intersection have short to no setbacks, contributing to a village feel.



Setbacks of intermediate depth create opportunities to add enhanced streetscape features.

Existing Setbacks Map



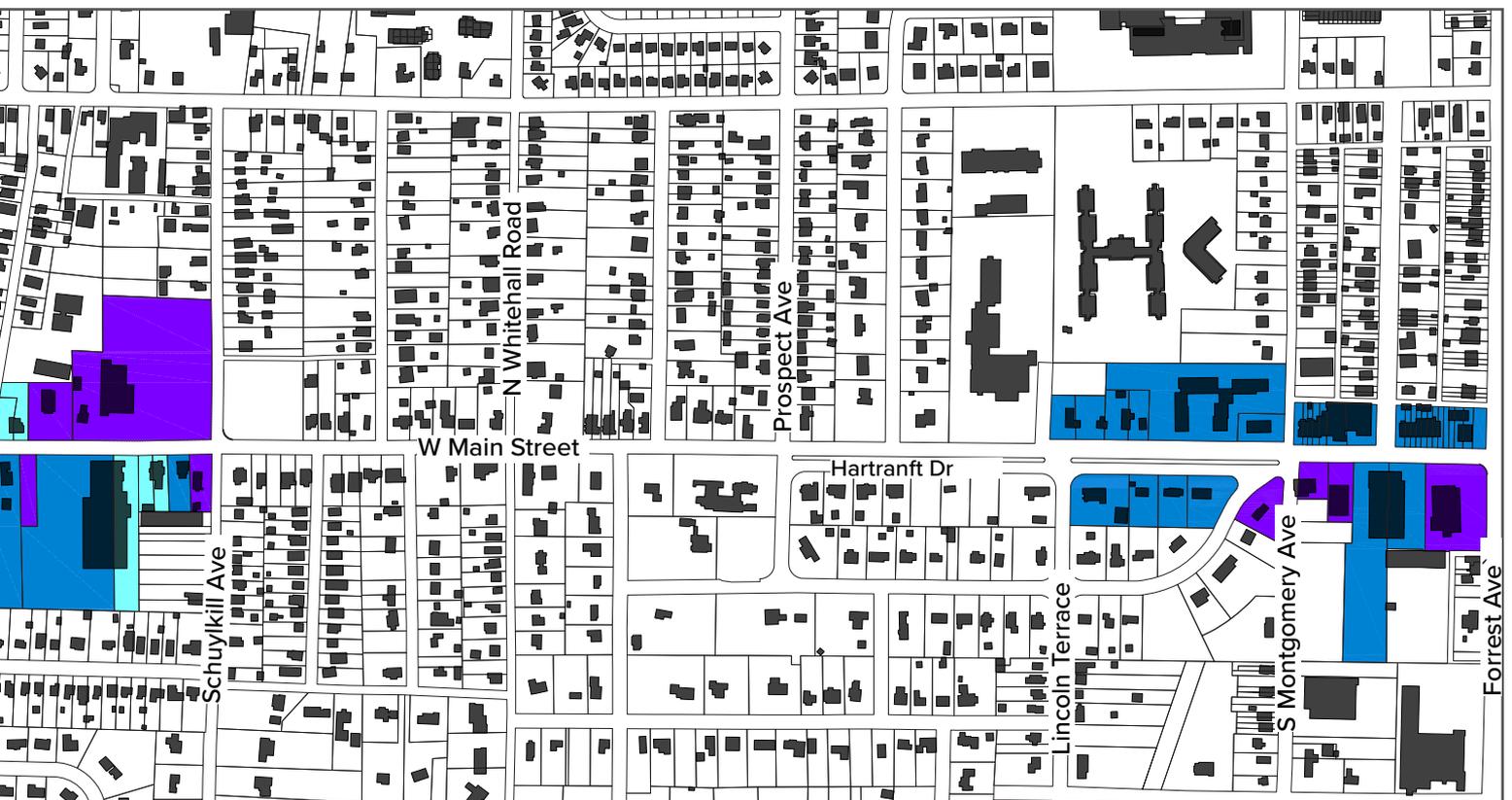


Shorter setbacks are fully occupied by auto merchandise with no room for sidewalks or landscaping.



Deeper setbacks result in rows of parking in front of buildings that are not visually or physically buffered from the pedestrian realm.

- 51' or more
- 36' - 50' Setback
- 21' - 35' Setback
- 0' - 20' Setback



MAIN STREET DEVELOPMENT FOCUS AREAS

Focus areas were identified based on a combination of all of the existing conditions to determine the areas of greatest need, opportunity, and impact. These are areas that would benefit the most from intermediate improvements to improve the look, feel, and function of the corridor. These areas could also serve as key redevelopment areas should the properties undergo redevelopment. Many of these properties could have a significant impact on the corridor should they be improved upon or redeveloped, especially those properties located at key intersections.

The map below identifies prime opportunity areas where investment would have the greatest impact.

Areas highlighted in red indicate prime opportunity areas for intermediate improvements or full redevelopment. These are areas that are lacking in pedestrian amenities, landscaping, and have poor access management. Intermediate improvements incorporated into those designated areas would have the greatest impact with minimal modifications to provide short-term improvements to the corridor and improve safety. Prime opportunity areas are also locations where anticipated future redevelopment will occur. If and when these properties are redeveloped, it is important that the guidelines recommended in this plan are incorporated into the site plans to contribute toward achieving the goals of creating a cohesive corridor. Efforts to coordinate with property and business owners in these focus areas should be a high priority for the township, and more resources should be concentrated in these areas to provide the greatest investment impact.

Focus Area Map





Areas highlighted in orange and yellow are areas where intermediate improvements would still have positive benefits, but existing conditions already provide basic pedestrian amenities and limited landscaping. These are also areas that are less likely to see redevelopment. However, these properties represent significant blocks along the corridor connecting the key intersections and strongly contribute to the overall function and feel of West Main Street. Pedestrian improvements, especially along Jeffersonville Golf Course, would improve connectivity and access between Burnside Avenue and Potts Avenue.

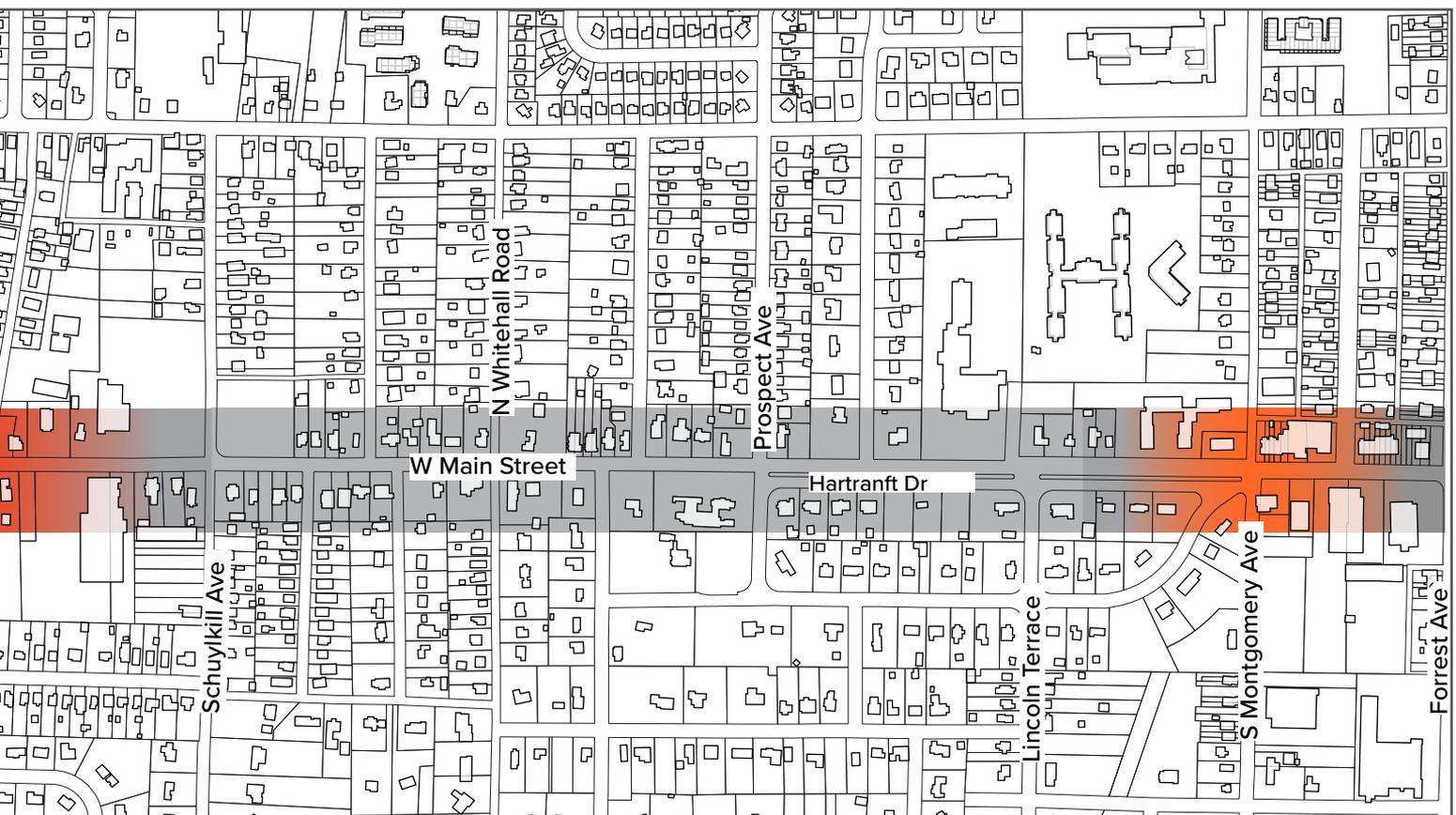


Areas highlighted in gray are areas where intermediate improvements would be least likely to provide many benefits, and where it is unlikely that redevelopment will occur. These are primarily the residential areas between Montgomery Avenue and Schuykill Avenue, and the properties adjacent to Trooper Road that have recently been redeveloped.



Limited
Intervention

Opportunity
Areas



The following guidelines are design treatment recommendations for the West Main Street Corridor. Each set of guidelines is tailored to the existing conditions, lot constraints, and desired architectural and development patterns. Some guidelines, such as the street cross-sections, vary based on where properties are located along the corridor, and are more influenced by surrounding development patterns. Others, such as pedestrian and architectural guidelines, are intended to apply to the entire length of the corridor as a way to create a cohesive flow of development, as well as a consistent and connected network of pedestrian amenities.

Additionally, some of these guidelines will only be possible to implement if a property were to be fully redeveloped. Building setbacks and, to a certain extent, parking configurations are challenging to modify on existing properties. However, guidelines for access management, pedestrian standards, and landscaping standards can be incorporated into existing properties to provide immediate improvements to the safety and function of the corridor. Property owners should be encouraged to implement intermediate improvements to the greatest extent possible that are feasible for their specific properties, with the future intent that redevelopment activities will incorporate all of the recommended design guidelines.



CHAPTER 4



CORRIDOR PLAN

RECOMMENDED DESIGN GUIDELINES

Property Frontage Setbacks

To provide nuanced development guidelines for the corridor, consideration was given to existing development patterns and future potential of roadway segments along West Main Street to develop three unique setback standards. Each of the setback scenarios considers existing property sizes and how future development would impact the overall look and function of the corridor. Each scenario establishes uniform setbacks and parking orientation for identified areas along the corridor. It is recommended that any future changes to zoning along the corridor incorporate these setback standards.

50 FOOT SETBACK

To take advantage of the deep parcel sizes and to remain consistent with recent redevelopment on the south side of West

Main Street near the Trooper Road intersection, a 50 foot setback is recommended for those properties identified in orange.

35 FOOT SETBACK

For most properties on the north side of West Main Street from Trooper Road to Jefferson Avenue, a 35 foot setback is recommended. Reducing setbacks for these properties would allow greater flexibility in relocating parking areas to the side and rear of buildings, and may also facilitate shared parking between properties. Traffic congestion often results in traffic back-ups in front of these properties, further hindering access and safety. Improved access management and better defined driveway entrances would limit vehicular conflicts and reduce the number of curb cuts.

Proposed Setback Map



20 FOOT SETBACK

Most properties are recommended to have a 20 foot setback. This designation is appropriate especially for those properties that are proximally located to key intersections and residential areas, but also for those properties that have shallow parcel sizes which do not support deeper setbacks. Existing development at the Forrest Avenue intersection is a continuation of the more urban development on Main Street in Norristown, and the Egypt Road intersection is reminiscent of a “village” style development that is highly conducive to a walkable, pedestrian-oriented business district.

Throughout the corridor, 6 foot wide sidewalks are recommended, with a 3 foot verge to buffer pedestrians from moving traffic.

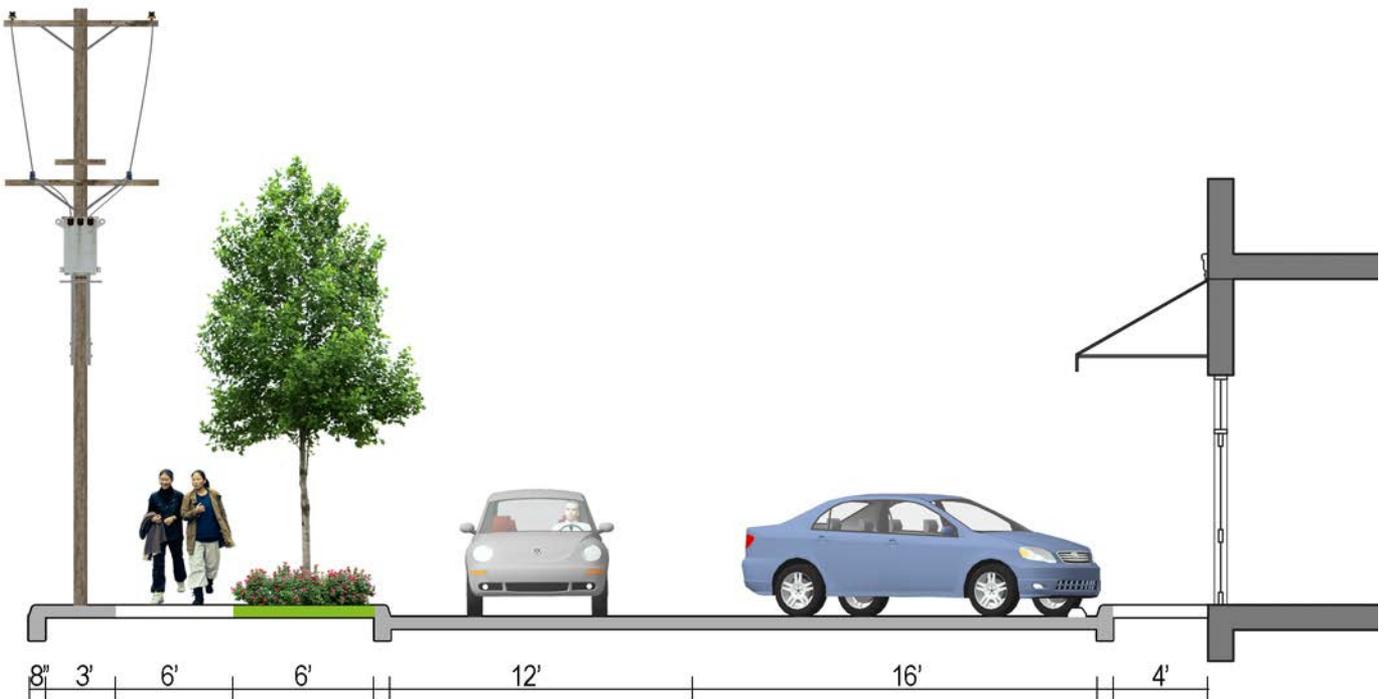
This orientation would ensure obstacles like utility poles are located outside of the sidewalk areas, providing clear access for pedestrians. Landscaping is also recommended for each of the setback scenarios based on the driveway curb cuts and parking orientation. Landscaping would be required around sidewalks, parking areas, and buildings to screen parking areas, provide shade, and add color and visual interest along the corridor. Landscape buffers, in addition to providing beautification, can also be designed to serve as stormwater management facilities to capture, filter, and temporarily store runoff from the adjacent development.

- 50' Setback
- 35' Setback
- 20' Setback
- Residential to remain



50' Commercial Setback

Properties utilizing a 50 foot setback would be able to incorporate a one-way drive aisle and angled parking in front of the building. A sidewalk adjacent to the building provides safe pedestrian access to the business entrances, and reduces the need for pedestrians to walk between parked cars. A landscape buffer would separate the front sidewalk from the drive aisle, provide increased separation for pedestrians, and create an opportunity to screen the drive aisle and parking row from the roadway. Street trees would be set back away from the overhead utility lines to reduce conflicts with tree branches and support healthy tree canopy growth.



35' Commercial Setback

Properties utilizing a 35 foot setback would be set slightly closer to the roadway but would still provide separation between buildings, vehicular travel lanes interior to the site, if they are present, and the pedestrian sidewalks. The front yard could be used as a full vegetated area, outdoor dining, patio, or as a one-way drive aisle in front of the building to facilitate circulation around the site and provide access to parking areas located on the side or rear of the building. Pedestrian sidewalks in front of the building would provide access to businesses, and a landscaped buffer would separate the drive aisle from the front sidewalk. A verge would provide buffering between the front sidewalk and moving traffic on West Main Street. Street trees would be set back away from the overhead utility lines to reduce conflicts with tree branches and support healthy tree canopy growth.



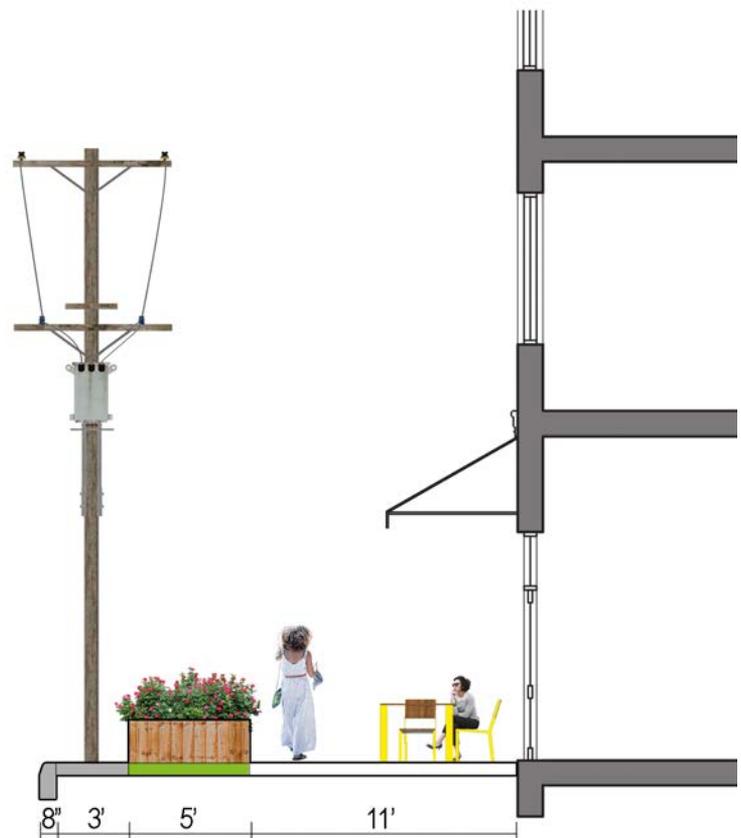
20' Commercial Setback

Properties utilizing a 20 foot setback would have all parking areas located to the side and/or rear of the building. A verge would provide buffering between moving traffic on West Main Street and the front sidewalk, while foundation plantings and a landscaped buffer would contribute to an attractive streetscape. Pathways connecting the front sidewalk to the building would provide entrances to businesses, while other entrances could be provided on the rear side of the building that faces the parking area. Street trees would be set back away from the overhead utility lines to reduce conflicts with tree branches and support healthy tree canopy growth.



20' Mixed-Use Setback

Properties utilizing a 20 foot setback could incorporate an outdoor retail or dining component into the streetscape by shifting the location of the landscaped buffer to be directly adjacent to the bricked verge, thereby directly linking the pedestrian sidewalk to the building entrances and outdoor patios. A wider sidewalk area can host outdoor displays of goods and merchandise, and can be used for café seating. A landscaped buffer or planters can be used to provide a buffer between the sidewalk and café area and moving traffic. Street trees, if incorporated, would need to be selected for a suitable height at maturity to avoid growing into the overhead wires.



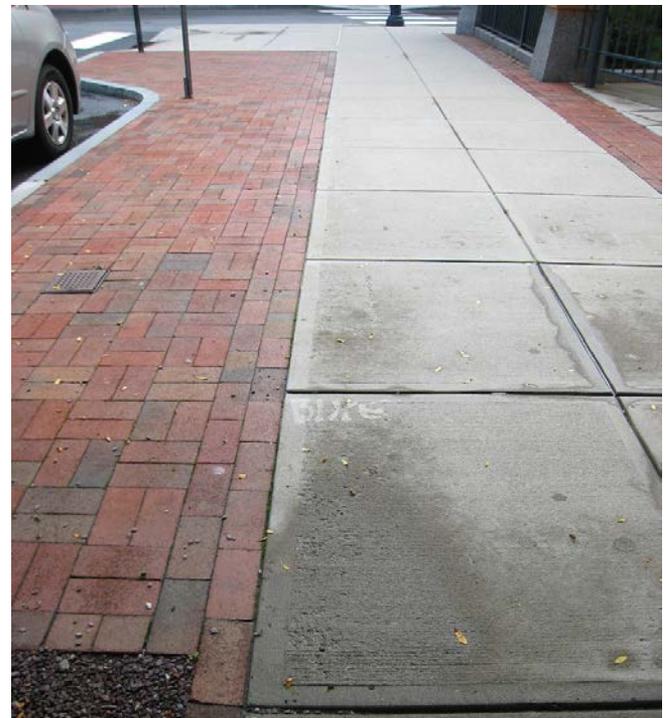
Pedestrian Standards

Throughout the entire corridor, a full and connected sidewalk network is recommended. Existing zoning standards in the R-TMO District require a minimum of four foot wide sidewalks separated from the curb line by a minimum of five feet of grass and landscaped area. However, this landscaped area can become overgrown or patchy, and lack of maintenance can detract from the overall appearance of the corridor. It is recommended instead that all new sidewalks should be at least 6 feet in width with a 3 foot verge paved with brick separating the sidewalk from the curb line. The intent behind this change is that it still maintains a total of nine feet of area from the curb line to the inside edge of the sidewalk, as currently required under the existing overlay standards, but it effectively creates a wider sidewalk that is more useable across its full width. Pedestrians are able to walk on the bricked area if the sidewalks are crowded, can wait for the bus on a more solid surface than a grassy area, and the visual break between the roadway and the sidewalk is maintained because of the change in the paving material. A brick verge is also easier to maintain than a vegetated verge, and will result in a more uniform streetscape. Reduced green space will be mitigated by landscape buffers on the inside of the sidewalk in front of parking areas or buildings.

Sidewalks are also recommended adjacent to all buildings of at least four feet in width for properties utilizing a 35 foot or a 50 foot setback standard. This will allow for pedestrians to safely navigate from their vehicles to building entrances out of the way of moving traffic or parking areas.



Above, existing conditions result in utility poles blocking sidewalk access and limit sidewalk utility. Below, an example of a bricked verge shows how additional pedestrian space can be created while still creating separation between the sidewalk and the roadway.



Architectural Guidelines

Community preferences indicated a desire to see a uniform building style to connect businesses along the corridor. Brick or other natural building materials are preferred over vinyl or metal for façades. Building façades should therefore adhere to the standards presently found in the R-TMO District:

“[buildings] shall...provide a combination of masonry materials, such as stone, stucco, or brick in combination with wood or simulated wood siding, and decorative elements around windows and doors, such as columns, pediments, and shutters, and new roof plan where flat roofs presently exist.”

Whereas the existing standards apply to the renovation of existing structures, it is recommended all new development should also adhere to the same architectural standards. Additionally, similar decorative treatments should apply to other streetscape features such as lighting and signage. Street lighting should be consistent throughout the corridor. The antique-style light posts installed at the development at West Main Street and Trooper Road should serve as a model for future development.

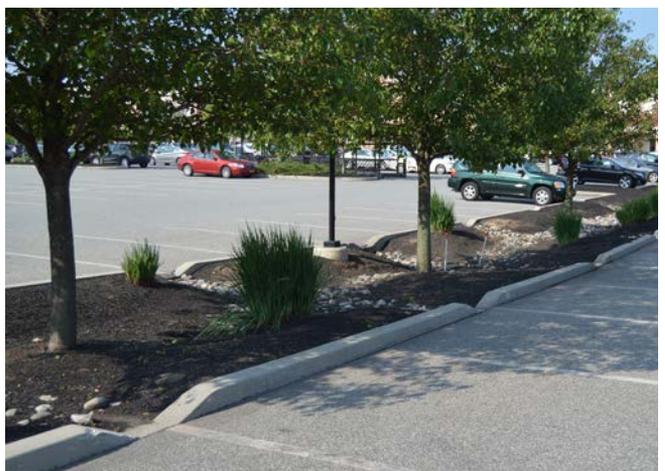
Uniform sign standards should be adopted for the corridor to ensure continuity throughout the corridor. Monument signs are preferred, using a base of brick or other masonry materials. On buildings themselves, blade signs are preferred over signs that are mounted flush with the building façade to make them easier to identify from a passing vehicle or as a pedestrian.



Colorful awnings draw greater attention to building tenants, and monument-style signage allows greater visibility to passing motorists.

Landscape Guidelines

All new developments will be required to install landscaping in keeping with the design guidelines. Landscaping buffer areas should be planted with a mix of shade trees, shrubs, and other grasses and perennials. Shade trees should be selected based on size at maturity, to ensure they are appropriate to be located under utility wires. Shrubs should be selected for their ability to provide an appropriate level of screening of drive aisles and parking areas. Landscaping plans should also consider how stormwater management elements could be implemented to capture and filter stormwater runoff from adjacent parking areas and building coverage. Where a full landscaped buffer is not feasible, decorative planters and/or hanging baskets should be installed.



Grasses and trees provide greenery to the streetscape while also providing buffering between the sidewalk and the parking area.

Landscaping pits and container landscaping are suitable for properties with short setbacks. Depressed curbing provides stormwater management in larger parking areas.

Proposed Access Management Plan

Under the setback standards, only properties utilizing a 50 foot setback would be permitted to have parking in front of buildings. These parking spaces would be angled and accessed only by a one-way drive aisle. Otherwise, all parking would be required to be located to the side and/or rear of the building. Additionally, no parking spaces would be permitted to have direct access onto West Main Street.

Existing properties should consider how to create shared parking, and all future development would be required to provide shared parking and access easements with neighboring properties to create connections with adjoining properties and enable access to the parking areas between properties.

Implementation of improved access management designs would contribute toward closing redundant or excessive curb cuts and create a more organized and safer flow of traffic. Installation of curbing and sidewalks would have multiple safety benefits: not only would the sidewalk network be strengthened and connected, but these kinds of improvements would establish more clearly defined curb cuts for businesses, further define traffic circulation patterns within and between businesses, and reduce potential vehicular conflicts.



Existing properties have wide, undefined curb cuts and large parking areas in front of the buildings. This reduces opportunities for shared parking and disassociates businesses from West Main Street.

Proposed Access Management Plan

To demonstrate how the pedestrian, parking, and landscape guidelines could be incorporated into the existing corridor, the following access management plan is proposed. This plan shows how the installation of a connected sidewalk network with a bricked verge, combined with improved landscaping and the closure of redundant or excessive curb cuts can transform West Main Street without requiring large-scale redevelopment. Where properties have the ability to create shared parking access, or

where properties have long and ill-defined curb cuts, this plan shows the closure of these curb cuts, identified by the orange symbol. As properties experience redevelopment or where reinvestment occurs, the township can use this access management plan to encourage property owners to meet the proposed site frontages shown. Additional discussion on intermediate improvements of properties, including access management strategies, is discussed in the next chapter.



1. Trooper Road to Rose Avenue and Jeffersonville Golf Club





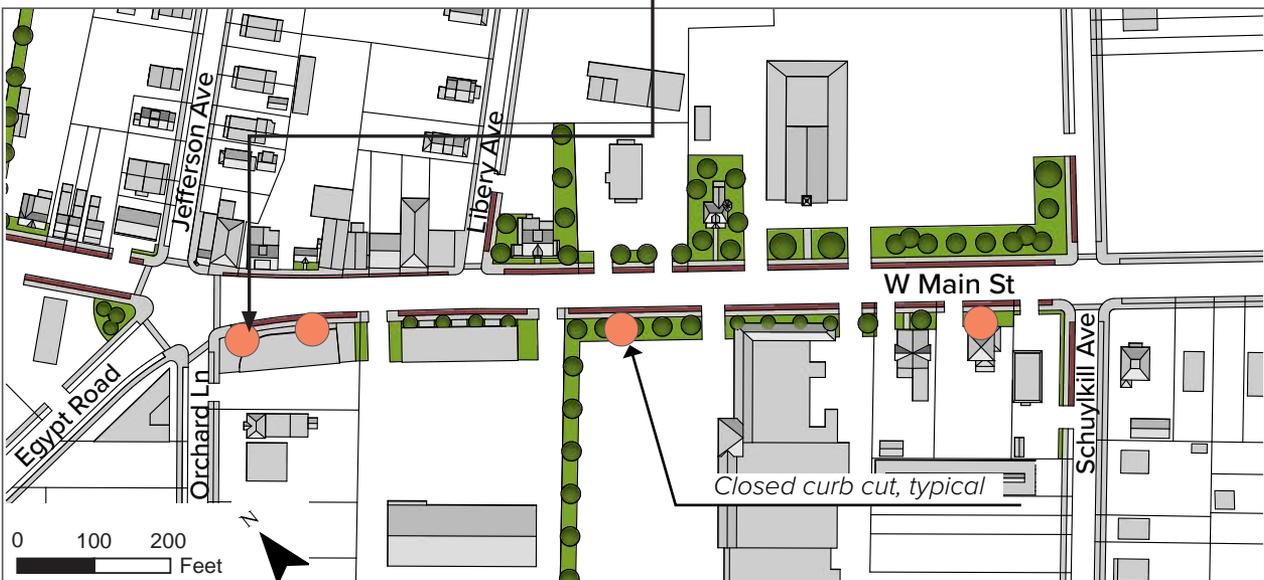
2. Rose Avenue to Burnside Avenue



Proposed Access Management Plan



4. Egypt Road Intersection to Whitehall Road



3. Burnside Avenue to Egypt Road Intersection



5. Montgomery Ave to Forest Road



INTERMEDIATE IMPROVEMENTS

Unless properties undergo significant redevelopment, it is unlikely many will be able to incorporate all of the suggested design guidelines. However, property owners may choose to implement intermediate improvements to begin to incorporate recommended designs, especially those relating to sidewalks, landscaping, and access management to provide interim improvements to safety, traffic patterns, and visual appearance. Coordination with PennDOT and SEPTA will be essential to implement any changes that impact curb cuts, roadway improvements, or transit stop locations. Property owners are also encouraged to coordinate with the township to discuss improvement opportunities at the early stages of site planning.

Following are some examples of existing properties with some of the recommended design elements incorporated into the current conditions to demonstrate the impact moderate investments can have on the corridor. Intermediate improvements can take the form of temporary or low-cost solutions like turf grass and planters, or can represent more permanent improvements, such as installation of curbing and sidewalk. This guidebook does not create a mandate or a requirement that property owners implement these suggestions, but rather intermediate improvements should be installed based on individual property needs and level of investment interest on behalf of the property owner.

Akshay Indian Grocery 2431 W Main Street

The grocery store currently has an ill-defined property frontage with a large pole sign and parking in front of the building. The wide-open curb cut allows vehicles free movement to turn into and out of the property at any point which could lead to vehicular conflicts. The lack of sidewalks also provides little protection for pedestrians, and no landscaping is present to soften or buffer the parking area from the roadway.

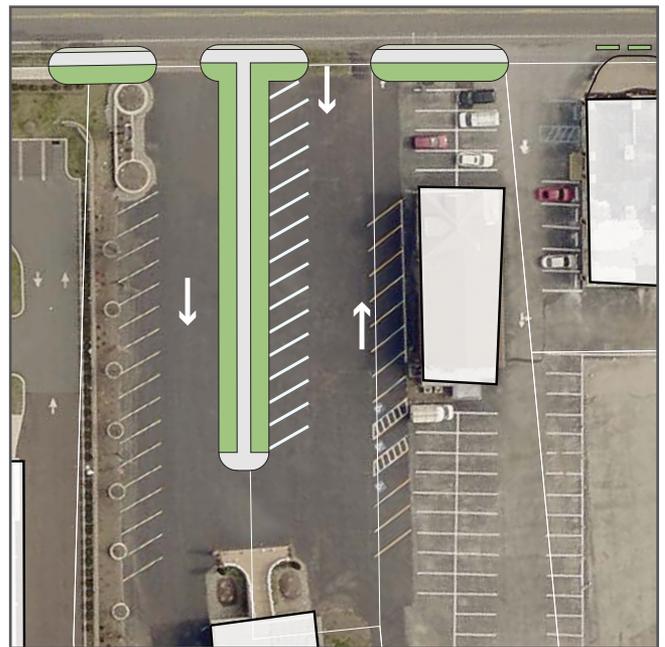
The suggested improvements include the installation of a section of curbing and sidewalk with a landscaped buffer along the main property frontage. This would limit uncontrolled ingress and egress from the parking lot and leave open two curb cuts on either side of the sidewalk section for improved traffic flow. Benches provided on either side of the existing ornamental fountain provide much-needed seating at an existing bus stop location, and provide increased safety and visibility for riders waiting to catch the bus. Alternatively, a bus shelter or other decorative canopy can be provided here to further enhance the bus stop.



Jalsa Indian Cuisine 2522 W Main Street

This restaurant and events center currently has a very deep setback for its building, with long rows of parking in the front. A partial sidewalk crosses over one driveway curb cut but does not continue the length of the frontage, and a limited grass buffer separates the far edge of the parking lot from West Main Street. One of the drive aisles is extra wide and overall the parking lot is vast and uninterrupted by any landscaping features.

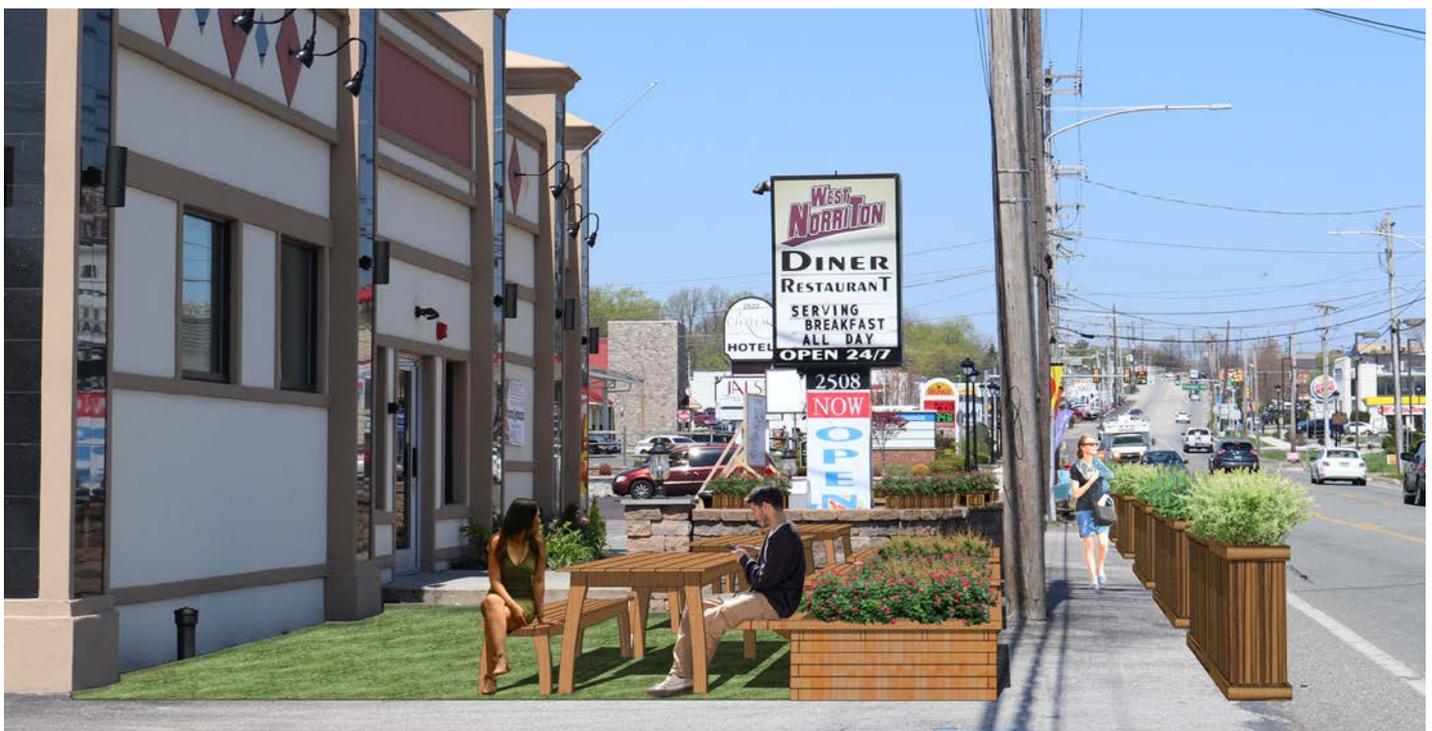
The suggested improvements include the installation of a landscaped pedestrian pathway connecting the front building entrance to the sidewalk. This grand boulevard draws attention to the ornate entry staircase. Canopy trees in the median would provide shade for parked cars and beautification to the site, while shrubs and grasses in the landscaped beds could serve as stormwater management controls. Dedicated one-way arrows would streamline traffic circulation within the site, and reduced width drive aisles would be more space efficient.



West Norriton Diner Restaurant 2508 W Main Street

The Diner currently has limited outdoor seating in front of the building surrounded by a low wall. However, no curbing or sidewalks exist to define the edge of the property and utility poles are located in an area where sidewalks would go. Vehicles are able to turn into the property anywhere along the frontage, and there is limited directional signage for vehicles.

The suggested improvements include planter boxes to define the property frontage, and create a buffered sidewalk area for pedestrians. The outdoor seating area has been expanded and additional planter boxes add color and visual interest.



Burnridge Center 2100 W Main Street

This shopping center currently has no sidewalk amenities, landscaping, or appropriate access management controls. There are three curb cuts that provide access into the site. One of the curb cuts on West Main Street is located close to the intersection and can lead to vehicular conflicts, especially considering the high volume of traffic at Burnside Avenue. A large paved area in the corner of the lot does not contribute to the parking or circulation of the lot and is generally underutilized.

The suggested improvements include the closure of one of the curb cuts, the installation of a bricked verge and sidewalk, and the creation of a landscape buffer. Relocation of the shopping center sign and angled parking establishes a better use of the corner space while maintaining the same number of parking spaces. Replacing the existing pole sign with a monument sign provides the opportunity to better advertise the names of the businesses in the shopping center. The landscaped buffer maintains visibility into the site while providing improved aesthetics and increased separation between the sidewalk and the parking area.



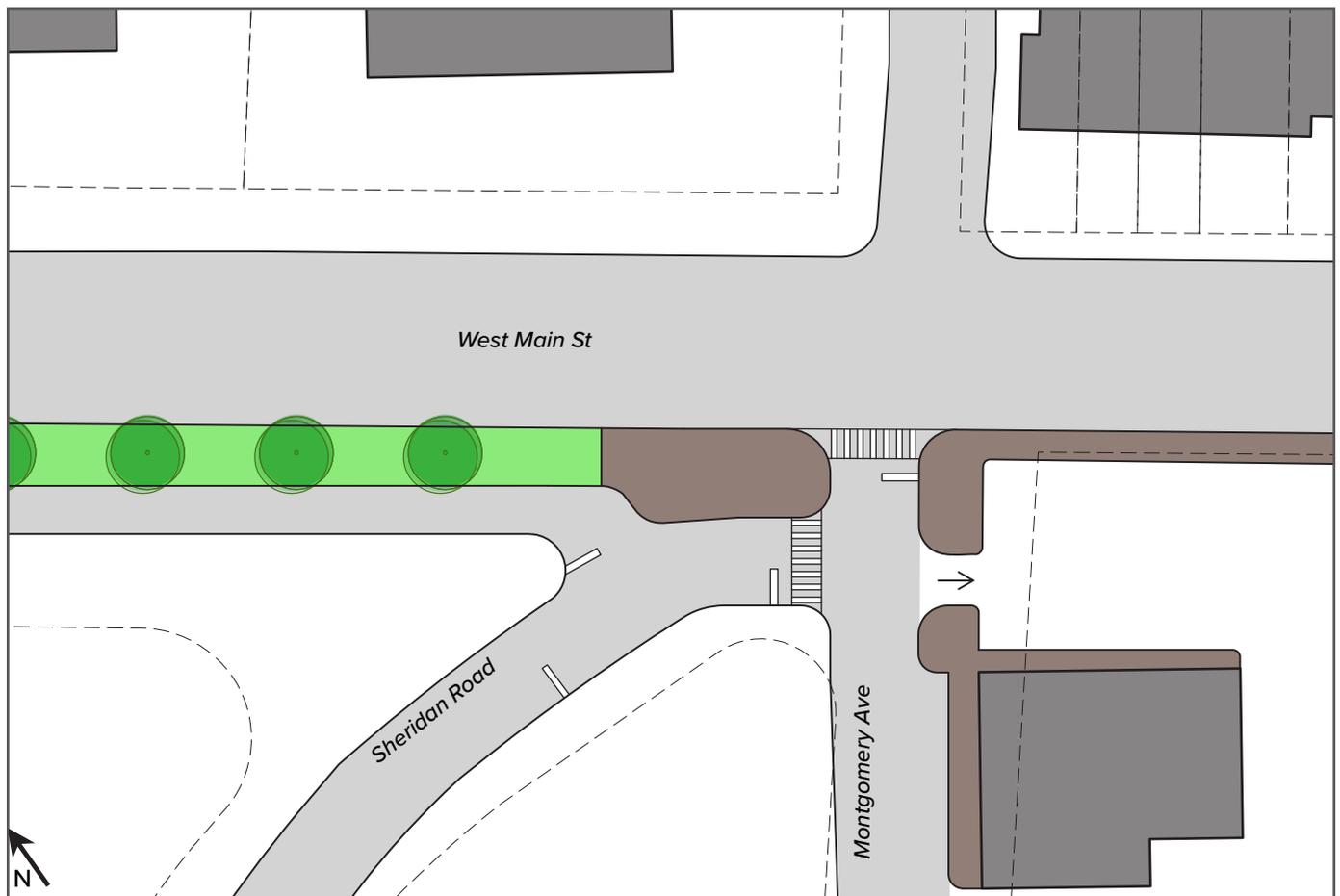
Montgomery Avenue Intersection

This intersection has numerous turning movements and very wide turning angles, leading to an ill-defined pattern of movement for cars and pedestrians. Additionally, the business on the southeast corner has a driveway entrance into and out of their property close to the intersection, which can create conflicts with turns into and out of the residential streets. An extension of the curbing to create “bump-outs” would narrow the roadway to create more clearly defined roadway entrances and would reduce the crossing distance for pedestrians. Painted crosswalks over the streets would provide increased visibility for pedestrians.



Aerial, 2017

Proposed Plan | 1" - 50'



Egypt Road Intersection

The Egypt Road intersection is a priority area for redevelopment, particularly those properties in the southeast corner. This intersection has been identified as a prime opportunity area, and it is recommended these properties utilize a 20' setback to emulate existing development patterns on the north side and support a "village" feel at this location. Ideal redevelopment for these sites would consist of mixed-use buildings that create new retail and commercial storefronts to the street level, with offices or residential uses in the upper floors. Parking should be located to the rear of the buildings and shared access should be allowed to create a flow of traffic through to the side streets. Coordinated development among multiple property owners will also need to consider the future road widening project of West Main Street proposed by PennDOT, which will likely affect the property frontages on the south side of the street. Connected sidewalks, landscaping, and attractive architecture including awnings and blade signs would further enhance this intersection and create a destination point in the heart of the township.



Aerial, 2017



Street View - looking west

The absence of utility poles on the south side of West Main Street at this location creates an opportunity to plant trees at 35 foot intervals in tree pits along the sidewalk. A bricked verge creates a visual separation from the roadway, and street trees provide shade and ambiance for pedestrians and outdoor dining tables.



Potential Schematic Plan



CHAPTER 5

PLAN VISION

Additional goals beyond the design guidelines were identified through the public outreach process. These goals are summarized here with specific implementation objectives. The township should consider these goals as decisions are being made that would impact the zoning code and subdivision and land development ordinance (SALDO), and other pedestrian, streetscape, and access management policies.

Goal: Encourage new businesses to locate along the West Main Street corridor and help existing businesses flourish.

Objective: Update the zoning along the West Main Street corridor to strengthen the requirements of the R-TMO District and provide amendments as necessary. Consider replacing the underlying zoning and overlay districts with one new zoning district.

Objective: Update the West Norriton Township Subdivision and Land Development Ordinance to include provisions that would create new landscaping and streetscape improvements as properties redevelop along the corridor.

Objective: Consider a management model, such as a “Main Street” or a “Business Improvement District” to provide additional support to business and property owners, and to coordinate business promotion and physical corridor improvements.

Goal: Encourage a diverse mixture of businesses and residential uses along the West Main Street corridor.

Objective: Review the permitted uses allowed in zoning districts located along the corridor. Expand the use of mixed-use buildings as a by-right use.

Objective: Partner with the Montgomery County Commerce Department to identify market needs and fill vacant properties with suitable tenants.

IMPLEMENTATION

Goal: Help decrease traffic congestion and improve safety along the route for pedestrians, cyclists, and motorists alike.

Objective: Require shared parking arrangements in the zoning districts in order to reduce the number of curb cuts and improve access management along West Main Street.

Objective: Continue coordinating with PennDOT about the future of the corridor and the lane expansion project. Discussion should be held about pedestrian safety and street crossings, including the possibility of pedestrian refuge islands and/or a landscaped median along portions of the roadway. Discussions should also be held about how to improve the safety of cyclists along the corridor, including the potential for bike lanes along the corridor.

Objective: Pursue sidewalk improvements along the entire corridor through grant funding opportunities and redevelopment. Six foot wide sidewalks with a three foot wide bricked buffer area between the sidewalk and the roadway should be included in the new SALDO standards.

Objective: Explore possibilities for trail connections from existing neighborhoods to the West Main Street corridor. Specifically, opportunities may exist to connect residents on Clemons Circle to West Main Street and to connect residents on Crawford Drive to Miami Road.

Objective: Focus on creating meaningful pedestrian connections to parks and recreation opportunities along the corridor, including Jeffersonville Golf Club, Burnside Park, and future open space associated with new development/redevelopment.

Goal: Create an attractive streetscape that is inviting to visitors and residents of West Norriton Township.

Objective: Pursue grant funding opportunities for streetscape improvements along the entire corridor and partner with businesses to make intermediate improvements, including new landscaping, street lighting, and sidewalk improvements to bring properties into conformance with the overall design standards identified for the corridor.

Objective: Update the West Norriton Township Subdivision and Land Development Ordinance to include provisions that would create new landscaping and streetscape improvements as properties redevelop along the corridor.

Objective: Explore the creation of a cohesive branding effort for the West Main Street corridor. This can include aspects such as uniform signage and banners, special events, social media, and local and regional advertising.

ORDINANCE AMENDMENTS

In order to achieve the stated results of corridor improvements, the following updates to the zoning code are suggested. Summaries of the recommended updates are included below; full text with suggested ordinance amendments are provided in the Appendix. The suggested zoning text amendments do not constitute a change to the existing zoning code, but merely represent changes necessary to revise the zoning district to better reflect the design standards suggested in this guidebook.

ZONING

The existing Revitalization and Transit Management Overlay District is an overlay district that layers additional standards on top of the underlying zoning districts for properties located in the commercial areas along West Main Street, and its standards are allowed by conditional use. To strengthen and incentivize the development of non-residential and mixed-use developments containing landscaping and site design standards in compliance with the design guidelines recommended in this document, it is recommended the overlay district either be allowed by-right rather than by conditional use, or the R-TMO should replace the underlying zoning districts in its entirety. Additional recommendations include the following:

- Strengthen pedestrian and vehicular circulation between properties by requiring cross-access connections.
- Application of the setbacks recommended in this document to reflect the existing patterns of development and desired function and aesthetics of the corridor.
- Reduce the option to locate parking in the front of buildings to improve circulation safety and access management to properties.
- Consolidate standards for sign regulations within §27-1601 Signs.
- Modify sidewalk standards to allow for greater sidewalk widths and bricked verges.

SUBDIVISION AND LAND DEVELOPMENT

The Subdivision and Land Development Ordinance (SALDO) should be strengthened to require streetscape elements recommended in this guidebook. For example, §22-505 requires minimum sidewalks of four feet in residential areas unless otherwise directed by the Board. Although specific requirements are provided for in the zoning code, the SALDO should have supporting and supplemental information to complement the zoning standards. Specifically, Part 5 Design Requirements should be considered as instrumental in laying out the framework for streetscape designs and elements that will contribute to a robust, pedestrian friendly street network. Sidewalk requirements, and others impacting driveway locations, number and width of curb cuts for properties, landscaping, parking lot orientation, and buffering, should be updated to provide more clear and specific requirements in alignment with those recommendations made in this guidebook.

CAPITAL IMPROVEMENT PLAN

The township should seek out means to provide recommended improvements along the corridor, and assist property owners in making improvements to their properties in accordance with the recommended design guidelines. Funding for the implementation of these recommendations may come from a variety of sources, and grant funding will be especially important to pursue, especially for facade improvements and other economic development strategies.

Township funding

- Capital Improvements Plan
 - Inclusion in the township's Capital Improvements Plan would enable a long-term savings plan to implement a specific project.
- Act 209 Fees
 - These fees are collected to be used toward specific intersection improvements identified in the township's Roadway Sufficiency Analysis and Capital Improvement Plan. Roadway improvements along West Main Street would contribute toward improving roadway safety along the corridor, and mitigate the negative impacts of congestion.

Grant opportunities

Numerous grant funding opportunities are available to assist the township in implementing the stated design goals for the West Main Street Corridor: Businesses on West Main Street may also wish to consider creating a Business Improvement District (BID) in order to build economies of scale for redevelopment efforts and increase funding capacity.

- Montgomery County
 - Montco2040 Implementation Grant
 - County Transportation Program
- DCED
 - Sponsors numerous programs targeting infrastructure, business attraction and retention including:
 - Business in Our Sites
 - Community Development Block Grant
 - Growing Greener II
- DVRPC
 - Additional grant funding can be pursued for intersection and roadway improvements:
 - Congestion Mitigation and Air Quality Program (CMAQ)
 - Transportation and Community Development Initiative (TCDI)

APPENDIX A

R-TMO District Amendments

PART 13B. REVITALIZATION AND TRAFFIC MANAGEMENT OVERLAY DISTRICT

§ 27-13B01. Legislative Intent.

[Added by Ord. 07-613, 3/13/2007, § 1]

It is the intent of the West Norriton Township Revitalization and Traffic Management Overlay District to serve as a mandatory overlay district by conditional use in order to eliminate blight, improve the overall appearance of the Township, improve traffic safety and reduce congestion. This district shall apply to any subdivision and land development application and shall not be limited by a grant of waiver of land development. It is the intent of this overlay district to meet the following objectives:

- A. To establish parking standards with the specific intent of coordinating traffic between adjoining properties.
- B. To reduce access points to improve traffic safety, circulation, and coordination.
- C. To provide standards for screening of serving and loading areas, HVAC equipment, and other areas determined to detract from the aesthetic qualities of the streetscape.
- D. To regulate outdoor dining activities for the benefit of customers and pedestrians, while also preserving required parking and safe vehicular circulation.
- E. To minimize the potential for increased traffic congestion by providing incentives that require shared access points, cross-access easements, shared parking areas, and quality public spaces.
- F. To increase the number of pedestrian and vehicular connections between adjacent properties to provide complimentary and coordinated development of adjacent properties.
- G. To provide regulating standards which require orderly, well-planned development and to ensure that the new buildings and additions enhance the surrounding streetscape, including incentives for burial of existing utilities within the right-of-way.
- H. To use scale, building orientation and landscaping to establish community identity.
- I. To effectively and efficiently regulate the establishment and maintenance of businesses requiring outdoor storage of vehicles, type and heights of signage, equipment or merchandise.
- J. To establish a walkable streetscape by promoting a pedestrian orientation of streets and buildings and providing a safe and convenient interconnected sidewalk network.

To ensure that increased standards of site design, pedestrian amenities, parking requirements, and landscaping features are incorporated into all developments along the corridor, it is recommended that this district replace the underlying zoning district, or allow the overlay standards by-right, to reduce additional hurdles and incentivize development in accordance with these standards.

While not located within the overlay ordinance, the underlying zoning districts (C Commercial and LC Limited Commercial) allow mixed-use buildings by special exception. If these districts remain as the base zoning districts, this requirement should be amended to allow these types of buildings by-right.

§ 27-13B02. District Established.

[Added by Ord. 07-613, 3/13/2007, § 1]

The West Norriton Township Revitalization and Traffic Management Overlay District shall be an overlay to the underlying zoning districts in the areas as depicted on the West Norriton Township Zoning Map. The following shall apply to the overlay district:

- A. The provisions and requirements of the West Norriton Township Revitalization and Traffic Management Overlay District shall be additional and supplemental to the underlying district provisions and uses.
- B. The West Norriton Township Revitalization and Traffic Management Overlay District shall apply as particularly identified on the West Norriton Township Zoning Map.

§ 27-13B03. Requirements for Conditional Use Application.

[Added by Ord. 07-613, 3/13/2007, § 1]

Any applicant desiring to improve property, submit a land development or perform an alteration to an existing building located in the West Norriton Township Revitalization and Traffic Management Overlay District is required to apply for and obtain conditional use approval pursuant to the provisions of § 27-1708 of this chapter, and said application shall be governed by the standards and criteria set forth § 27-13B04 below.

§ 27-13B04. Conditional Use Standards and Criteria.

[Added by Ord. 07-613, 3/13/2007, § 1; and amended by Ord. 07-617, 9/11/2007]

1. Applicants shall be required to pursue, where physically feasible, cross-easement agreements with neighboring property owners for the purpose of creating a cohesive and efficient parking configuration and traffic circulation plan.
2. Applicants shall be required to investigate the feasibility of and to reduce, to the extent possible, the number of existing curb cuts in order to improve traffic safety and circulation.
3. Appropriate fenced and/or landscaped screening shall be required around all HVAC equipment, service and loading areas, trash receptacles, and other areas deemed appropriate by the Board of Commissioners. A privacy fence, landscaped buffer and/or low shrubs shall provide screening along the side and rear property boundaries to residential and institutional zoning districts.
4. ~~To the extent feasible~~, applicants shall provide pedestrian and, ~~where feasible~~, vehicular connections with adjacent properties to facilitate circulation.
5. Applicants shall be required to investigate the feasibility of and, to the extent possible, consolidate two or more parcels, under separate ownership, prior to development, with the purpose of providing a more unified development.
6. The applicant shall appropriately landscape along the backside of the public sidewalk with low shrubs, ornamental walls and street trees ~~earth shaping~~. Where it is determined by the Board of Commissioners that insufficient space exists for such landscaping, they may be located elsewhere on the lot, at locations determined acceptable by the Board.

Recommend to allow developments under this overlay by-right. All subsequent standards listed in §27-13B04 would be required for all proposed developments.

Strengthen pedestrian connections between properties to improve walkability.

Or potentially a fee-in-lieu.

7. The applicant shall renovate existing building facades to provide a combination of masonry materials, such as stone, stucco, or brick in combination with wood or simulated wood siding, and decorative elements around windows and doors, such as columns, pediments, and shutters, and new roof plan where flat roofs presently exist.

8. The applicant shall bring the front facade wall and sidewalk into conformity with § 27-13B04, Subsection 17, contained herein.

9. Outdoor dining may be permitted, subject to the following:

A. The Board of Commissioners may limit the volume of music, hours of operation and outdoor lighting of the outdoor dining area so as to minimize its impact on neighboring properties.

B. Outdoor cooking shall not be permitted.

C. Planters, posts with ropes, iron fencing or other removable enclosures are encouraged and shall be used as a way of defining the area occupied as outdoor seating.

D. Refuse facilities shall be provided.

E. Advertising or promotional features shall be limited to umbrellas or canopies.

F. Outdoor dining shall not impede pedestrian traffic flow.

G. Floor area devoted to outdoor dining shall be provided with off-street parking in the same fashion as required for restaurant use at § 27-1410F(6) herein.

Only properties with a 50' setback would be allowed to have parking in the front. All other parking would be required to be located to the rear or side.

10. All outside lighting on the premises, including sign lighting, shall be arranged, designed and shielded or directed so as to protect the abutting streets and highways and adjoining property from the glare of lights, and lighting shall be so shielded that the source of the light shall not be visible from any point outside the premises. No flashing or intermittent or moving lights shall be permitted, either freestanding, attached to a facade, or as a part of an approved sign.

Mixed-use buildings require slightly taller ground floor heights to facilitate commercial uses. Maximum building height should not exceed 38'.

11. The following dimensional requirements shall apply to all properties within the Revitalization and Traffic Management Overlay District:

A. Maximum building height: three stories, not to exceed 35 36 feet.

B. Front, side and rear yard setbacks: The front, side and rear yard setbacks shall be no less than 10 feet, unless adjacent to an existing residential zoning district, in which case the minimum side and rear yard setbacks shall be 30 feet.

All setbacks should be amended to reflect the setback standards outlined in this document, based on the setback classification a property is designated.

C. Parking setback. All parking shall be set back a minimum of 10 feet from the ultimate right-of-way along all Ridge Pike frontages. The parking setback along all collectors or residential streets may be zero feet from the ultimate right-of-way. A minimum of 10 feet from the curbline shall be provided for the placement of sidewalks, landscaping and utilities.

The minimum setback distance for drive aisles would be 15' for properties with a 35' setback.

12. Occupation of ultimate right-of-way. Any right-of-way outside of the cartway shall be preserved for sidewalks and green area between the curblin and the front yard setback along the entire Ridge Pike, Egypt, Orchard and Trooper Road frontages.

13. Minimum green area: 15%, which may include any green area in the right-of-way as provided in § 27-13B04, Subsection 12. If 15% green area is physically impossible, the applicant shall make a contribution in lieu of green area to the Township. Such contribution will be based on the assessed value of the property.

14. A maximum of one thirty-foot curb cut per street frontage shall be provided, unless additional curb cuts are approved by the Township Commissioners in order to accommodate existing or proposed circulation deemed desirable by the Township, including installation of one-way movements limiting existing or proposed driveways to entrance or exit only. The applicant shall reduce the number and width of existing curb cuts when practicable.

15. The following sign standards shall apply in addition to the regulations set forth in Part 16, Signs, of the West Norriton Township Code, as amended. Where the regulations of Part 16 are in conflict with the standards of this section, this section shall control.

- A. No off-premises advertising shall be permitted.
- B. The maximum height of freestanding signs shall be 10 feet. Ground monument signs are ~~encouraged~~ **required**.
- C. The maximum height of wall-mounted signs shall be 15 feet, provided that in no event shall any portion of a sign extend above the roof eave line of the building to which the sign is attached.
- D. Illumination of signs shall be from white or amber color sources only. Illumination shall be from interior sources only, unless the Board of Commissioners shall approve lighting from exterior sources, which shall include a stipulated maximum wattage.
- E. Signs with moving parts or signs with changeable copy (including digital reader boards) and light sources which shall flash or pulsate more than once per 24 hours shall be prohibited, except for signs indicating time, date and weather.
- F. Signs shall not exceed 50 square feet per side.
- G. Signs shall be located behind (out of) the ultimate right-of-way of adjoining roads, except when a pillar agreement shall be agreed to between the applicant and the Township.

16. The Township Board of Commissioners may permit an applicant to bury all utilities along Ridge Pike frontage in lieu of payment of Act 209 traffic impact fees.

17. Pedestrian standards.

- A. Sidewalks are required to connect the street frontage sidewalks to all front building entrances, parking areas, central open space and any other destination that generates pedestrian traffic.

Provide additional details for parking lot landscaping and buffer landscaping to maximize the stormwater management potential and provide stronger standards for acceptable levels of screening and vegetation.

This section should be removed from the ordinance language and housed exclusively in Part 16 of the township zoning code, rather than having separate standards.

Current standards for illumination discourage internally illuminated signs in favor of external down-lighting sources, such as gooseneck lamps.

B. Sidewalks shall connect to existing sidewalks on abutting tracks and other nearby pedestrian destination points and transit stops.

C. Striped crosswalks shall be installed at any major intersection, ~~at the discretion of the Township Board of Commissioners.~~

D. Sidewalks shall be a minimum of ~~four~~ **six** feet in width and separated from the curbline by a minimum of ~~five~~ **three** feet of ~~grass and landscaped~~ brick area, which shall, in any event, comply with the design standards contained herein, ~~unless specifically waived by the Board of Commissioners.~~

E. All sidewalks and pedestrian connections shall be located a minimum of five feet from any buildings to allow for landscaping, unless arcades or entryways are part of the facade.

It is proposed that sidewalks shall be 6' in width with a 3' bricked verge between the sidewalk and the curbline, rather than grass or other vegetation. To build a connected sidewalk network, this requirement should never be waived.

18. Hotel use permitted.

A. On lots having a minimum area of three acres, a hotel may be permitted, provided that the following regulations in addition to those required by the Revitalization and Traffic Management Overlay District shall also apply:

(1) The lot shall contain a minimum of 100 feet of frontage along Ridge Pike.

(2) The maximum height of a hotel shall not exceed five stories nor be greater in overall ~~building height~~ than 65 feet.

(3) Any building used in whole or in part for hotel purposes shall be set back a minimum of 100 feet from a residential zoning district.

(4) Public sanitary sewer and water, or an alternate system approved by the Board of Commissioners, shall be provided.

(5) Where the hotel use shall be provided with shared parking opportunities with adjoining properties or uses, such as restaurant, office building, retail store or banquet facility, then the required parking per each rental unit shall be one parking space; otherwise the regulations of Part 14, § 27-1410, shall apply. Accessory activities such as dining, snack or exercise facilities that are limited to use by hotel lodgers shall not require additional parking.

(6) During the consideration of the conditional use approval, the applicant shall provide plans and procedures to be approved by the Board of Commissioners, which shall demonstrate satisfactory provisions for firesafety, emergency access, and emergency evacuation.

This should be optional, to allow for a choice to have foundation landscaping or not in front of buildings. All buildings though should have pedestrian walkways separating building entrances from any vehicular areas.

19. The Board of Commissioners may modify or waive specific requirements of this section as well as the requirements of Part 14, General Regulations, § 27-1410, Required Off-Street Parking and Loading, if the Board determines an improved development plan or better vehicular or pedestrian circulation will result.

APPENDIX B

Community Survey Response Summary